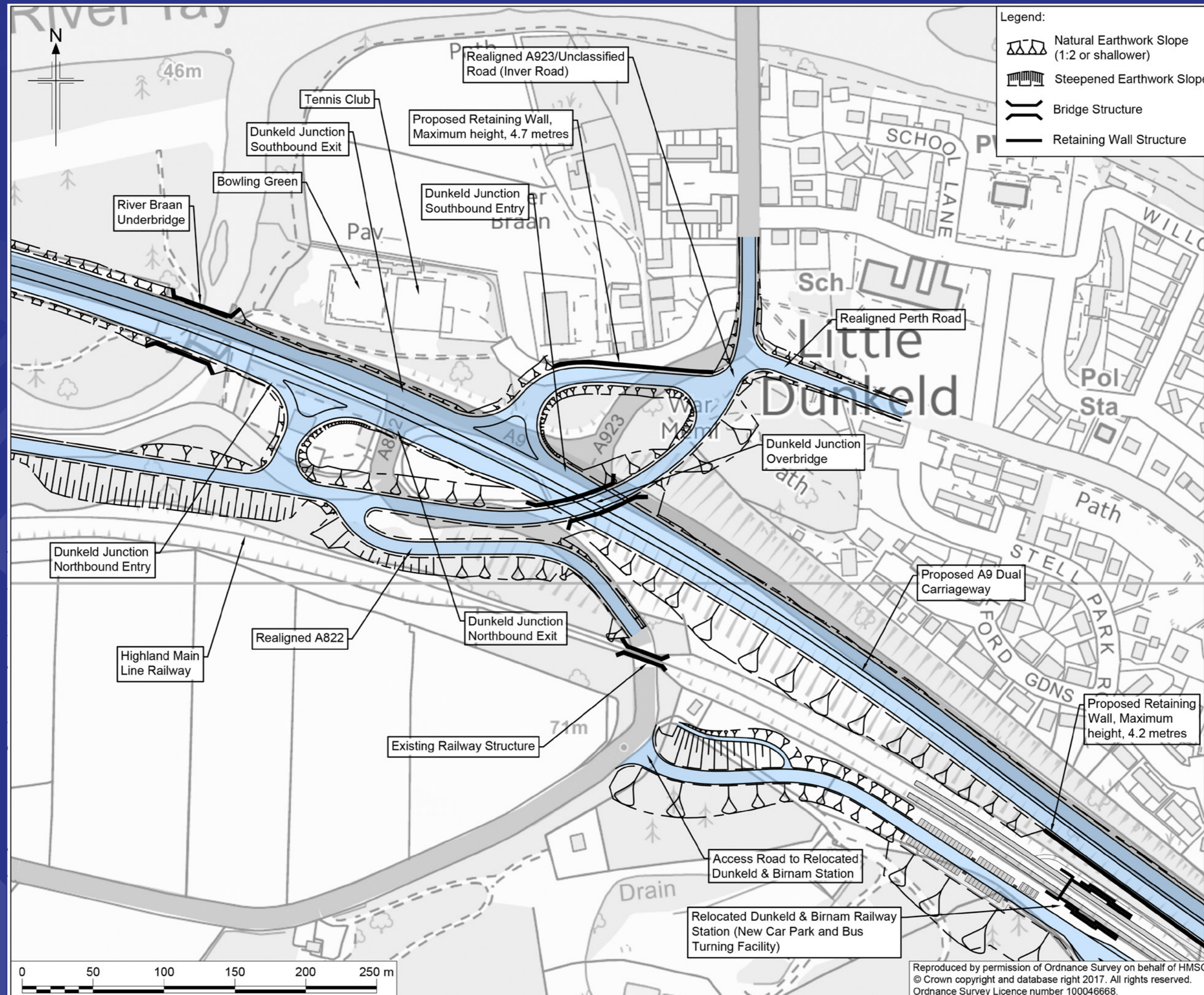


Main features:

- Provides full access to and from the A9.
- Layout not recommended for use on Category 7A dual carriageways.
- Inconsistency of junction type throughout the A9.
- Good quality side road connection between the A923 and Unclassified Road (Inver Road).
- A9 generally at existing carriageway level.
- No impact on residential properties.
- New bridge over River Braan (Special Area of Conservation) similar level as existing A9 bridge.
- Provides journey time improvements due to 70mph speed limit.
- Improves access to the A9 for local road traffic.
- Retaining wall required alongside residential properties and Craiginvean Surgery.
- Impact on access arrangements to residential properties, Craiginvean Surgery, Tennis Courts and Bowling Green.
- Steepened earthworks required alongside Highland Main Line railway, potentially introducing landscape and visual impacts.
- Bridge over A9 required, introducing landscape and visual impacts.
- Unacceptable gradient over 12% required on realigned A923/Unclassified Road (Inver Road).
- Estimated traffic flows suggest type of junction is unsuitable.
- Layout incorporates unacceptable reduced horizontal and vertical curvature standards and forward visibility.
- Station relocation proposed, with access from the A822. Alternative station design (e.g. layout on plan reference: B2140002/SK/259) could also be used.



A9 Dualling Programme

Pass of Birnam to Tay Crossing

Dunkeld Junction Local Grade Separation I



Notes:

1. Design shown is a preliminary design and will be subject to further assessment and refinement to ensure compliance with relevant design standards.
2. Design shown is a reduced standard alternative option developed following feedback from the public regarding the scale of junctions proposed. It has been developed for discussion purposes only.
3. Only main features are noted for this option. This list is not exhaustive and there may be more local impacts associated with this option.