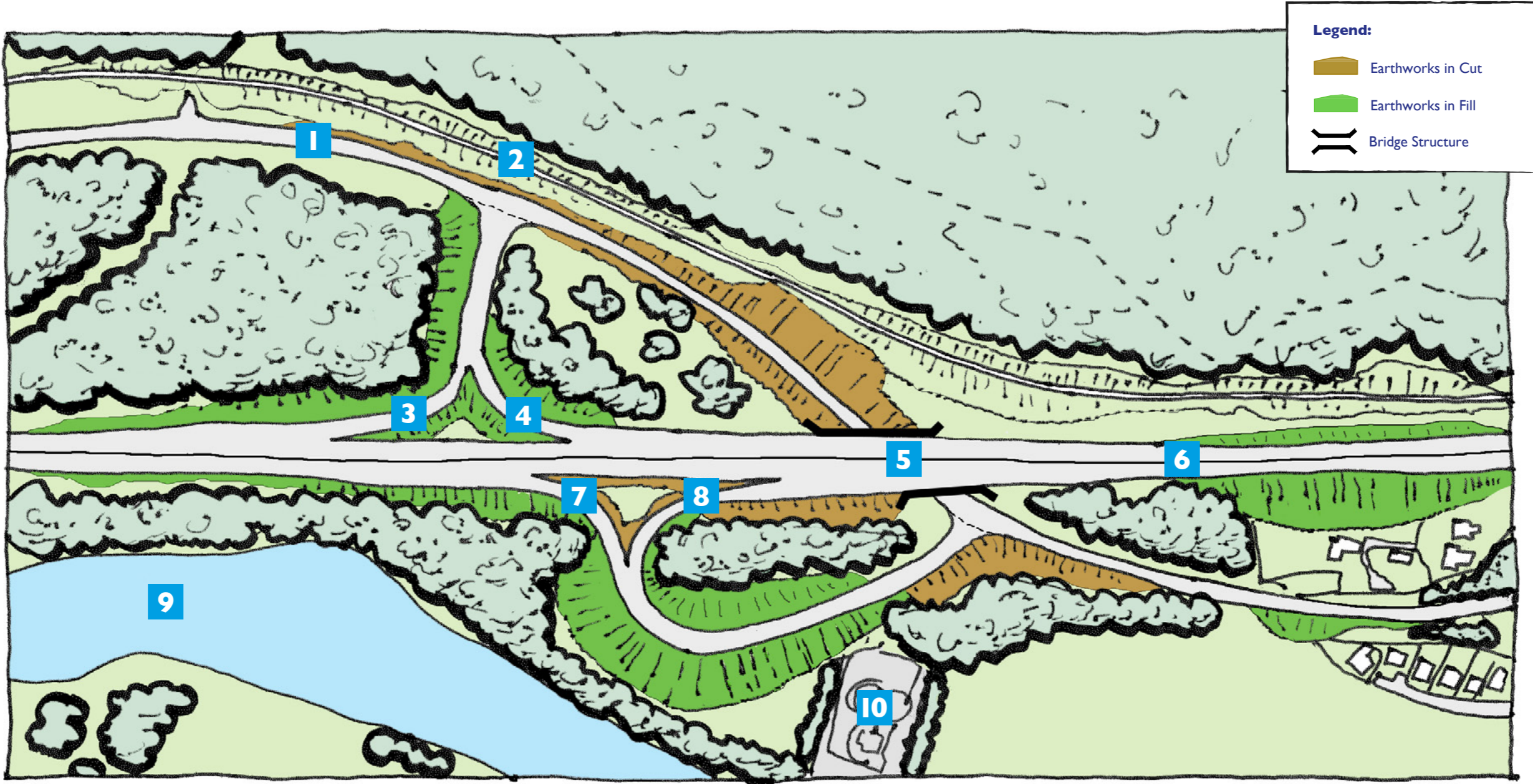


Main features:

- Provides full access to and from the A9.
- Layout suitable for Category 7A dual carriageways.
- Maintains continuity of junction type throughout the A9.
- Good quality side road connection between Perth Road and the B867.
- A9 generally at existing carriageway level.
- No impact on residential properties.
- No impact on sewage works.
- Land-take within River Tay Flood Zone.
- No impact on River Tay (Special Area of Conservation).
- Land-take within Ring Wood, impacting habitat for natural species and Ancient Woodland, introducing landscape and visual impacts.
- Steepened earthworks required alongside Highland Main Line railway, potentially introducing landscape and visual impacts.
- Steepened earthworks required alongside sewage works, potentially introducing landscape and visual impacts.
- Street lighting not required.
- Provides journey time improvements due to 70mph speed limit.
- Improves access to the A9 for local road traffic.

Birnam Junction Preliminary Drawing Option I



1 REALIGNED PERTH ROAD / B867

2 HIGHLAND MAIN LINE RAILWAY

3 NORTHBOUND EXIT SLIP ROAD

4 NORTHBOUND ENTRY SLIP ROAD

5 BIRNAM JUNCTION UNDERBRIDGE

6 PROPOSED A9 DUAL CARRIAGEWAY

7 SOUTHBOUND ENTRY SLIP ROAD

8 SOUTHBOUND EXIT SLIP ROAD

9 RIVER TAY (SPECIAL AREA OF CONSERVATION)

10 SEWAGE WORKS

A9 Dualling Programme

Pass of Birnam to Tay Crossing

Birnam Junction Preliminary Options Drawing 1/5



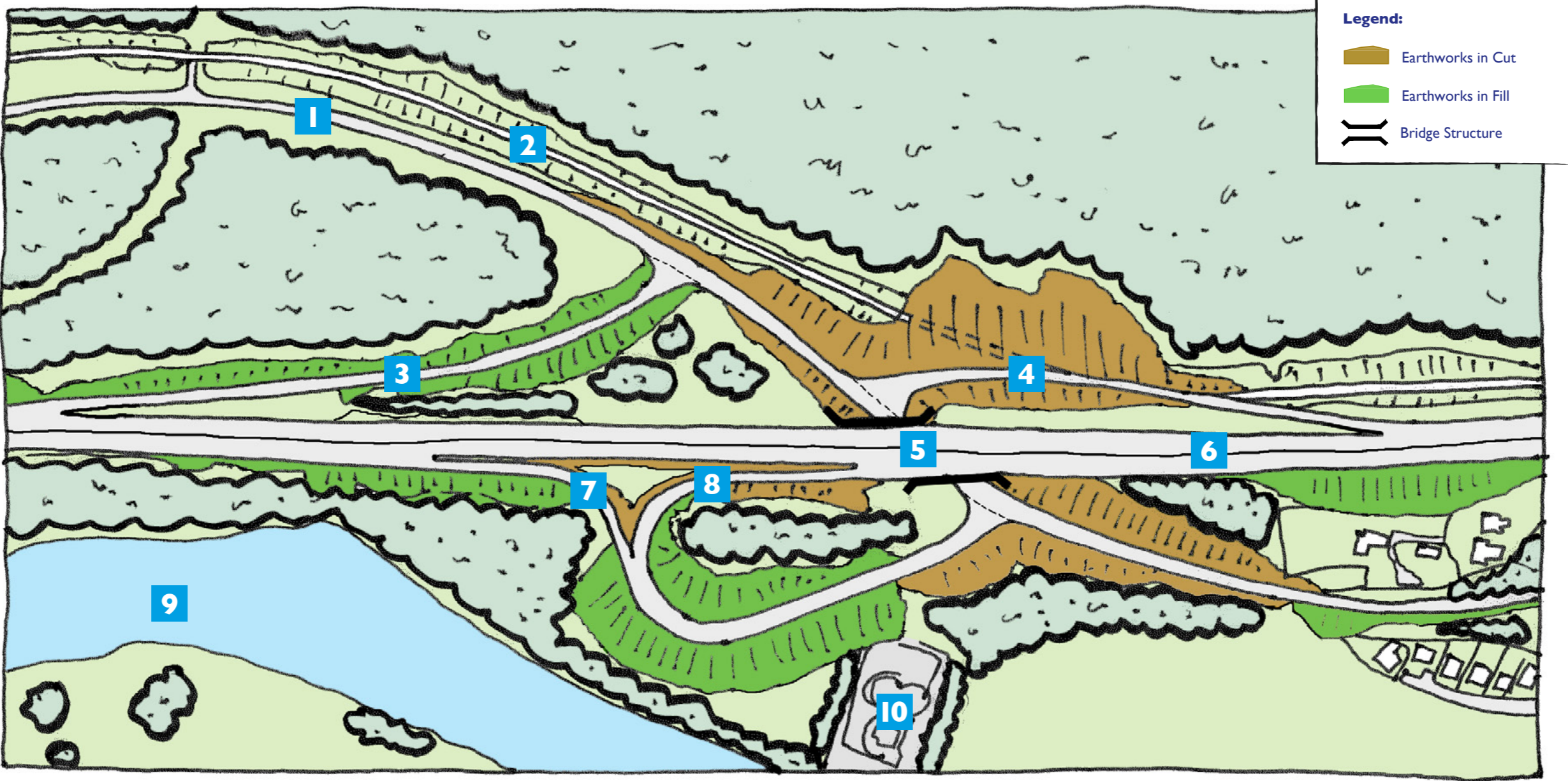
Notes:

1. Design shown is a preliminary design and will be subject to further assessment and refinement to ensure compliance with relevant design standards.
2. Design shown is a grade-separated junction option considered by Transport Scotland / Jacobs as part of a sifting process carried out prior to the DMRB Stage 2 assessment process.
3. Only main features are noted for this option. This list is not exhaustive and there may be more local impacts associated with this option.

Main features:

- Provides full access to and from the A9.
- Layout suitable for Category 7A dual carriageways.
- Maintains continuity of junction type throughout the A9.
- Good quality side road connection between Perth Road and the B867.
- A9 generally at existing carriageway level.
- No impact on residential properties.
- No impact on sewage works.
- Land-take within River Tay Flood Zone.
- No impact on River Tay (Special Area of Conservation).
- Land-take within Ring Wood, impacting habitat for natural species and Ancient Woodland, introducing landscape and visual impacts.
- Unacceptable impact on Highland Main Line railway.
- Steepened earthworks required alongside sewage works, potentially introducing landscape and visual impacts.
- Street lighting not required.
- Provides journey time improvements due to 70mph speed limit.
- Improves access to the A9 for local road traffic.

Birnam Junction Preliminary Drawing Option 2



1 REALIGNED PERTH ROAD / B867

2 HIGHLAND MAIN LINE RAILWAY

3 NORTHBOUND EXIT SLIP ROAD

4 NORTHBOUND ENTRY SLIP ROAD

5 BIRNAM JUNCTION UNDERBRIDGE

6 PROPOSED A9 DUAL CARRIAGEWAY

7 SOUTHBOUND ENTRY SLIP ROAD

8 SOUTHBOUND EXIT SLIP ROAD

9 RIVER TAY (SPECIAL AREA OF CONSERVATION)

10 SEWAGE WORKS



A9 Dualling Programme

Pass of Birnam to Tay Crossing

Birnam Junction Preliminary Options Drawing 2/5



Notes:

1. Design shown is a preliminary design and will be subject to further assessment and refinement to ensure compliance with relevant design standards.
2. Design shown is a grade-separated junction option considered by Transport Scotland / Jacobs as part of a sifting process carried out prior to the DMRB Stage 2 assessment process.
3. Only main features are noted for this option. This list is not exhaustive and there may be more local impacts associated with this option.

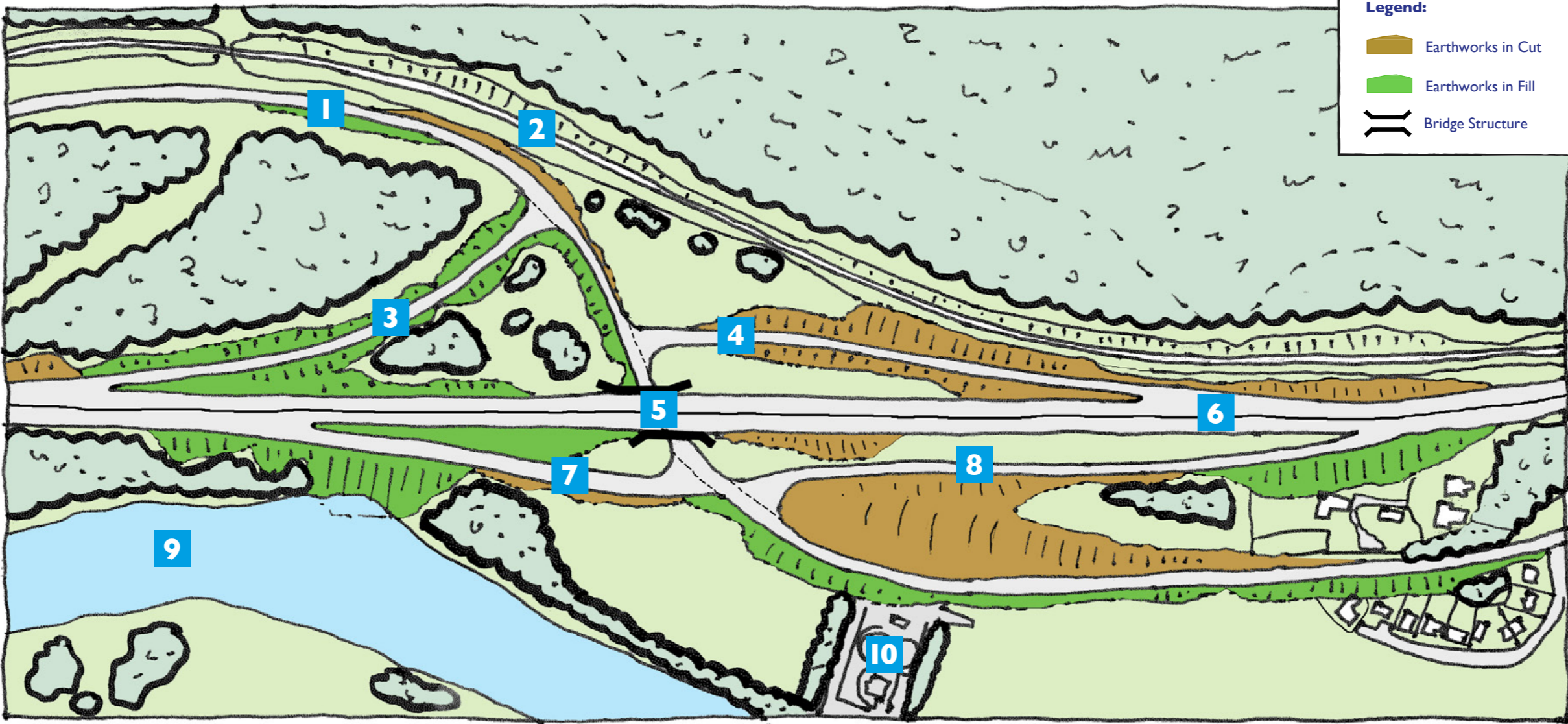
JACOBS



Main features:

- Provides full access to and from the A9.
- Layout suitable for Category 7A dual carriageways.
- Maintains continuity of junction type throughout the A9.
- Good quality side road connection between Perth Road and the B867.
- A9 generally at existing carriageway level.
- Impact on residential properties.
- Impact on access arrangements to sewage works.
- Impact on River Tay (Special Area of Conservation).
- Land-take within River Tay Flood Zone.
- Land-take within Ring Wood, impacting habitat for natural species and Ancient Woodland, introducing landscape and visual impacts.
- Impact on Highland Main Line railway.
- Steepened earthworks required alongside sewage works, potentially introducing landscape and visual impacts.
- Street lighting not required.
- Provides journey time improvements due to 70mph speed limit.
- Improves access to the A9 for local road traffic.

Birnam Junction Preliminary Drawing Option 3



- | | | | | |
|---------------------------------------|-------------------------------------|------------------------------------|---|--------------------------------------|
| 1 REALIGNED PERTH ROAD / B867 | 2 HIGHLAND MAIN LINE RAILWAY | 3 NORTHBOUND EXIT SLIP ROAD | 4 NORTHBOUND ENTRY SLIP ROAD | 5 BIRNAM JUNCTION UNDERBRIDGE |
| 6 PROPOSED A9 DUAL CARRIAGEWAY | 7 SOUTHBOUND ENTRY SLIP ROAD | 8 SOUTHBOUND EXIT SLIP ROAD | 9 RIVER TAY (SPECIAL AREA OF CONSERVATION) | 10 SEWAGE WORKS |



A9 Dualling Programme

Pass of Birnam to Tay Crossing

Birnam Junction Preliminary Options Drawing 3/5



Notes:

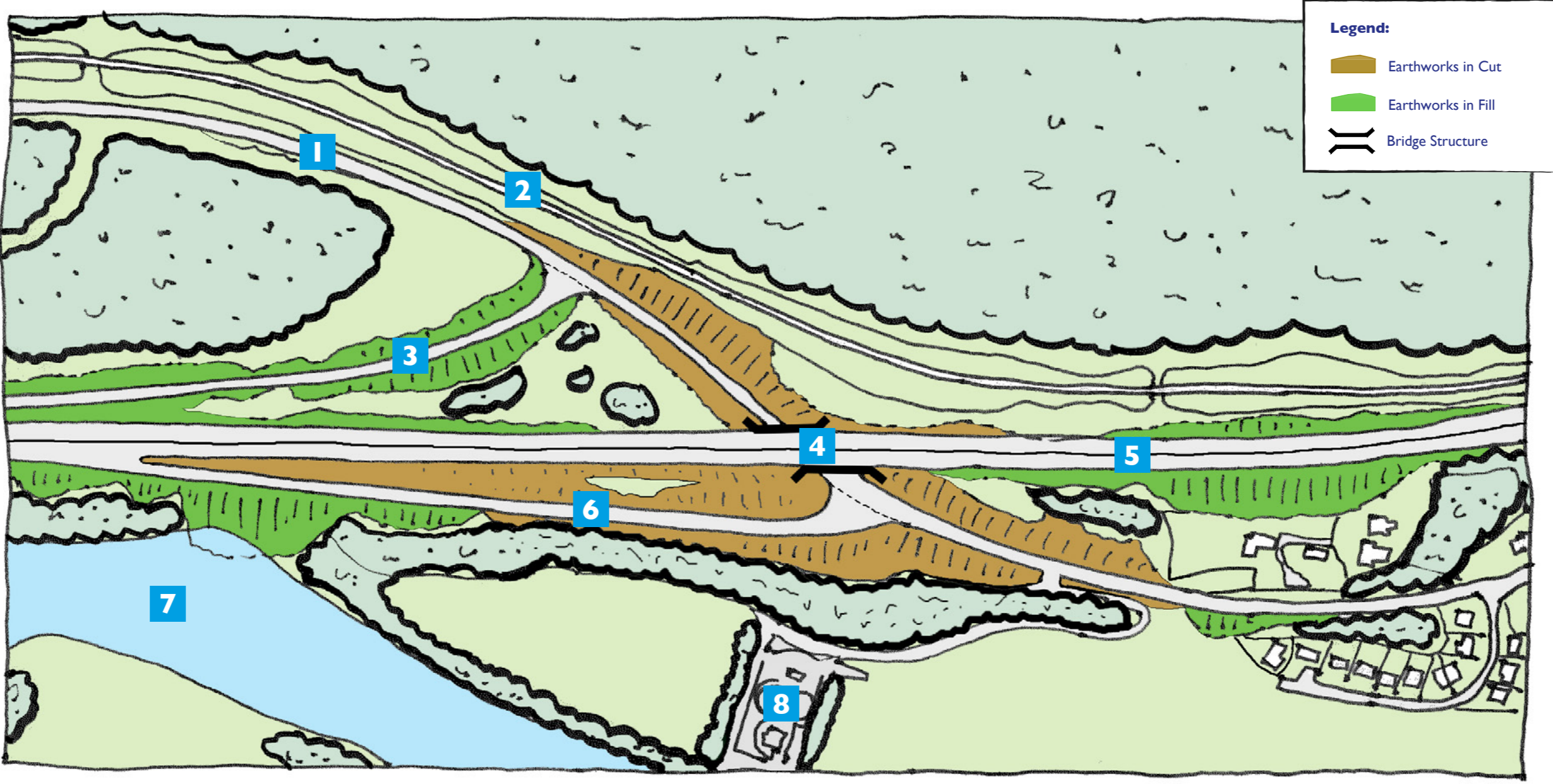
1. Design shown is a preliminary design and will be subject to further assessment and refinement to ensure compliance with relevant design standards.
2. Design shown is a grade-separated junction option considered by Transport Scotland / Jacobs as part of a sifting process carried out prior to the DMRB Stage 2 assessment process.
3. Only main features are noted for this option. This list is not exhaustive and there may be more local impacts associated with this option.



Main features:

- Provides full access to and from the A9.
- Layout suitable for Category 7A dual carriageways.
- Maintains continuity of junction type throughout the A9.
- Good quality side road connection between Perth Road and the B867.
- A9 generally at existing carriageway level.
- No impact on residential properties.
- No impact on sewage works.
- Impact on River Tay (Special Area of Conservation).
- Land-take within River Tay Flood Zone.
- Land-take within Ring Wood, impacting habitat for natural species and Ancient Woodland, introducing landscape and visual impacts.
- Steepened earthworks required alongside Highland Main Line railway, potentially introducing landscape and visual impacts.
- Street lighting not required.
- Provides journey time improvements due to 70mph speed limit.
- No access to/from the A9 to the north at Birnam Junction.
- Narrow footprint of dualling north of Birnam Junction as no slip roads provided adjacent to properties or railway.
- Increase in traffic flows on Perth Road (possibly by up to 75%, assuming full access at Dunkeld Junction).

Birnam Junction Preliminary Drawing Option 4



- | | | | |
|---------------------------------------|-------------------------------------|---|--------------------------------------|
| 1 REALIGNED PERTH ROAD / B867 | 2 HIGHLAND MAIN LINE RAILWAY | 3 NORTHBOUND EXIT SLIP ROAD | 4 BIRNAM JUNCTION UNDERBRIDGE |
| 5 PROPOSED A9 DUAL CARRIAGEWAY | 6 SOUTHBOUND ENTRY SLIP ROAD | 7 RIVER TAY (SPECIAL AREA OF CONSERVATION) | 8 SEWAGE WORKS |



A9 Dualling Programme

Pass of Birnam to Tay Crossing

Birnam Junction Preliminary Options Drawing 4/5



Notes:

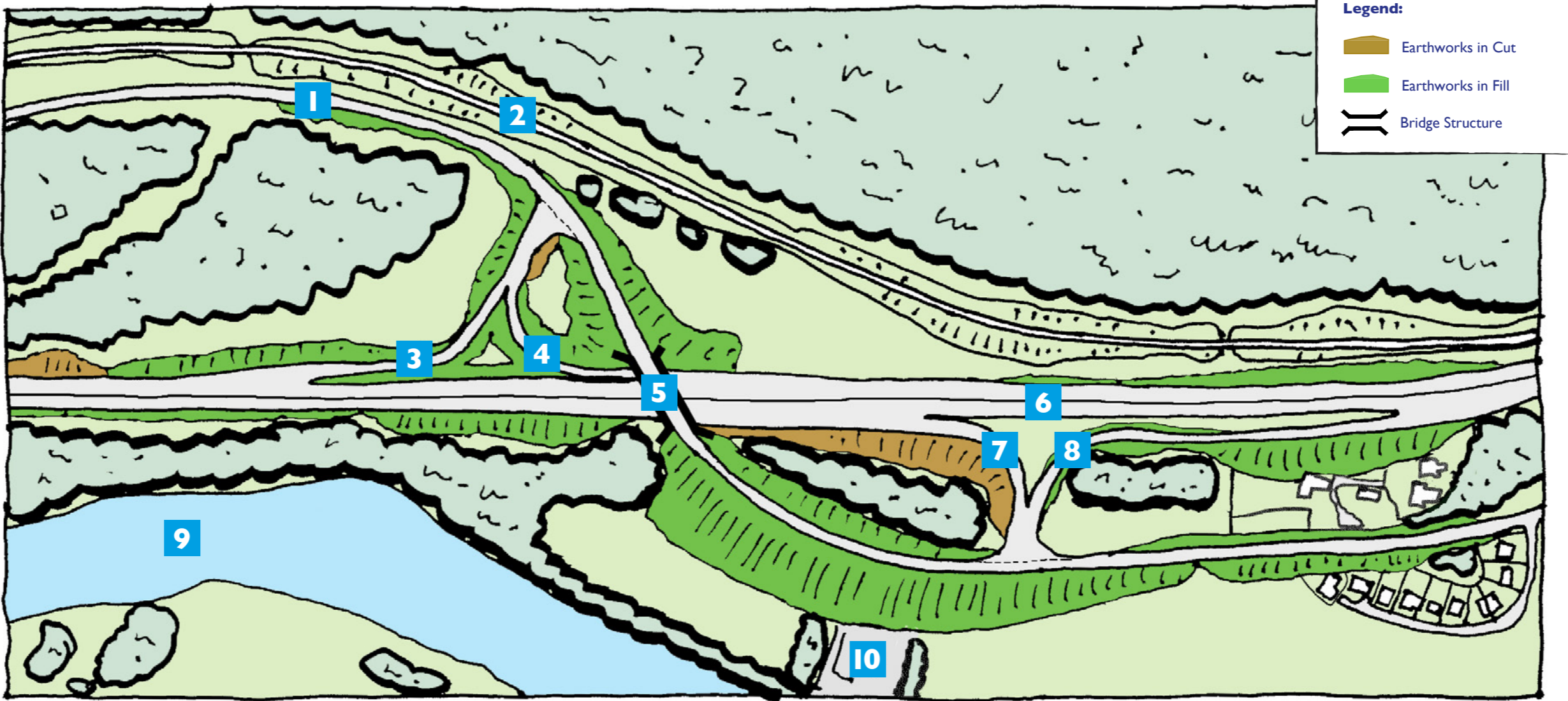
1. Design shown is a preliminary design and will be subject to further assessment and refinement to ensure compliance with relevant design standards.
2. Design shown is a grade-separated junction option considered by Transport Scotland / Jacobs as part of a sifting process carried out prior to the DMRB Stage 2 assessment process.
3. Only main features are noted for this option. This list is not exhaustive and there may be more local impacts associated with this option.



Main features:

- Provides full access to and from the A9.
- Layout suitable for Category 7A dual carriageways.
- Maintains continuity of junction type throughout the A9.
- Good quality side road connection between Perth Road and the B867.
- A9 generally at existing carriageway level.
- Impact on residential properties.
- Impact on sewage works.
- Land-take within River Tay Flood Zone.
- Land-take within Ring Wood, impacting habitat for natural species and Ancient Woodland, introducing landscape and visual impacts.
- Street lighting not required.
- Provides journey time improvements due to 70mph speed limit.
- Improves access to the A9 for local road traffic.
- No impact on Highland Main Line railway.
- No impact on River Tay (Special Area of Conservation).
- Short length of slip roads may introduce a safety issue during peak traffic periods.

Birnam Junction Preliminary Drawing Option 5



- | | | | | |
|---------------------------------------|-------------------------------------|------------------------------------|---|-------------------------------------|
| 1 REALIGNED PERTH ROAD / B867 | 2 HIGHLAND MAIN LINE RAILWAY | 3 NORTHBOUND EXIT SLIP ROAD | 4 NORTHBOUND ENTRY SLIP ROAD | 5 BIRNAM JUNCTION OVERBRIDGE |
| 6 PROPOSED A9 DUAL CARRIAGEWAY | 7 SOUTHBOUND ENTRY SLIP ROAD | 8 SOUTHBOUND EXIT SLIP ROAD | 9 RIVER TAY (SPECIAL AREA OF CONSERVATION) | 10 SEWAGE WORKS |

A9 Dualling Programme

Pass of Birnam to Tay Crossing

Birnam Junction Preliminary Options Drawing 5/5



Notes:

1. Design shown is a preliminary design and will be subject to further assessment and refinement to ensure compliance with relevant design standards.
2. Design shown is a grade-separated junction option considered by Transport Scotland / Jacobs as part of a sifting process carried out prior to the DMRB Stage 2 assessment process.
3. Only main features are noted for this option. This list is not exhaustive and there may be more local impacts associated with this option.